

7.0 Scale and Appearance

7.1. Massing & Storey Heights

The scale and massing of the proposed housing development are wholly appropriate to the context and location. The scheme has been carefully designed to respond to the character and grain of the surrounding built environment, ensuring a harmonious relationship with adjacent buildings and streetscapes. Heights and building volumes have been modulated to reflect prevailing patterns in the area, avoiding overdevelopment while making efficient use of the site.

The proposed massing respects existing sightlines, protects neighbouring amenity, and creates a comfortable human scale at street level. Variation in rooflines, articulation of façades, and thoughtful placement of openings contribute to a sense of rhythm and proportion that echoes local typologies. Furthermore, the development transitions sensitively in scale where it adjoins lower-rise properties, ensuring that it sits comfortably within its urban or setting.

In this way, the development supports both visual integration and contextual appropriateness, reinforcing local identity while accommodating much-needed housing in a form that is both contemporary and respectful.



Key

- 1 Storey
- 2 Storey
- 2.5 Storey
- 3 Storey

7.2. Scale against existing context

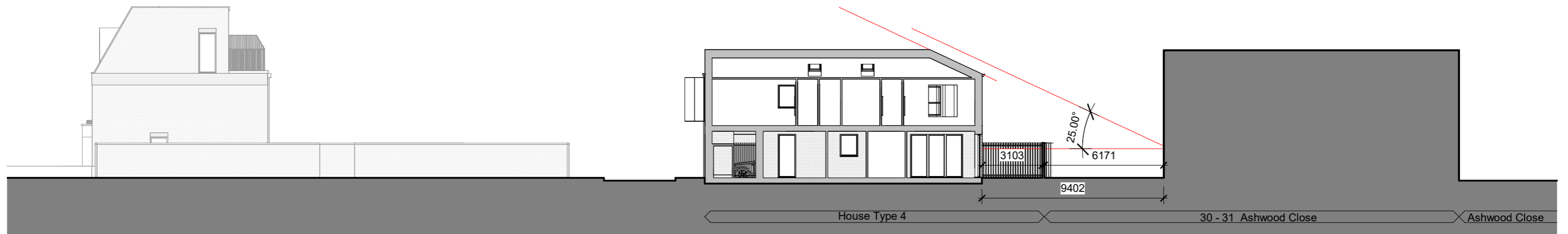
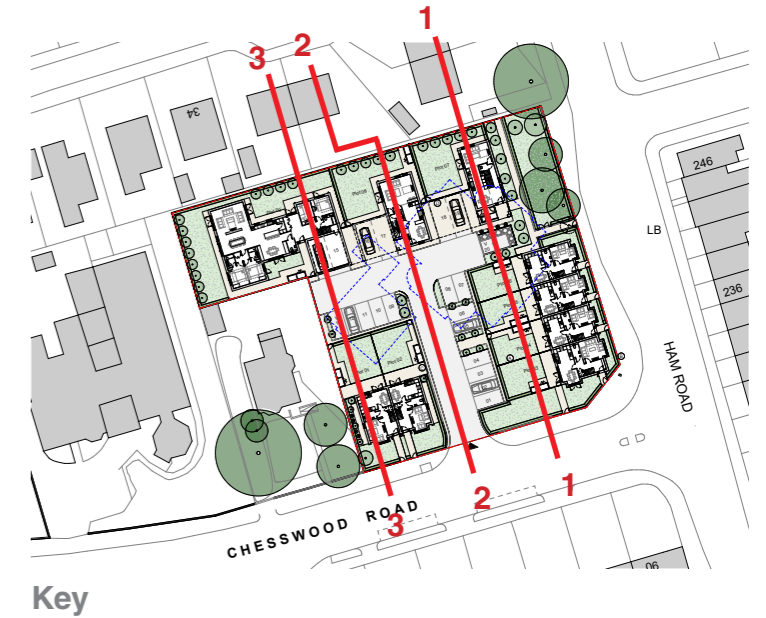
The proposed development has been carefully designed with due regard to the guidelines set out in the BRE publication “Site Layout Planning for Daylight and Sunlight: A Guide to Good Practice”. The scale, massing, and orientation of the buildings have been developed to ensure that acceptable levels of daylight and sunlight are maintained both within the new homes and for neighbouring properties.

Internal layouts have been optimised to maximise natural light, with primary living spaces positioned to benefit from favourable aspects. Window placement and façade articulation have been considered to enhance daylight penetration while mitigating potential issues such as overshadowing or excessive solar gain.

For neighbouring properties, the scheme has been tested to ensure that it does not result in unacceptable losses of light or breaches of BRE thresholds. Where potential impacts were identified, the massing has been adjusted to minimise overshadowing, particularly

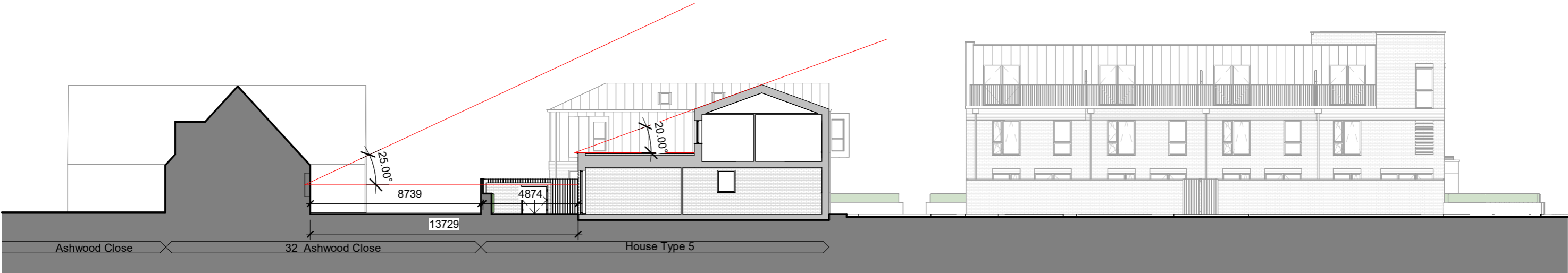
to gardens and windows serving habitable rooms.

Overall, the design represents a balanced response to site opportunities and constraints, delivering a high standard of amenity for future residents while safeguarding the daylight and sunlight levels of surrounding buildings. In this way, the proposal supports sustainable, healthy living environments and meets the expectations of good urban design.

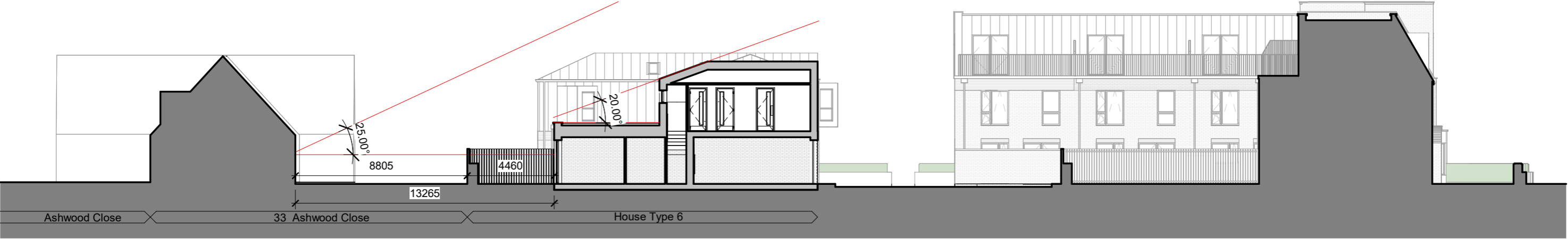


Section 1 through proposed House Type 4 and neighbouring building at Ashwood Close

7.3. Scale against existing context



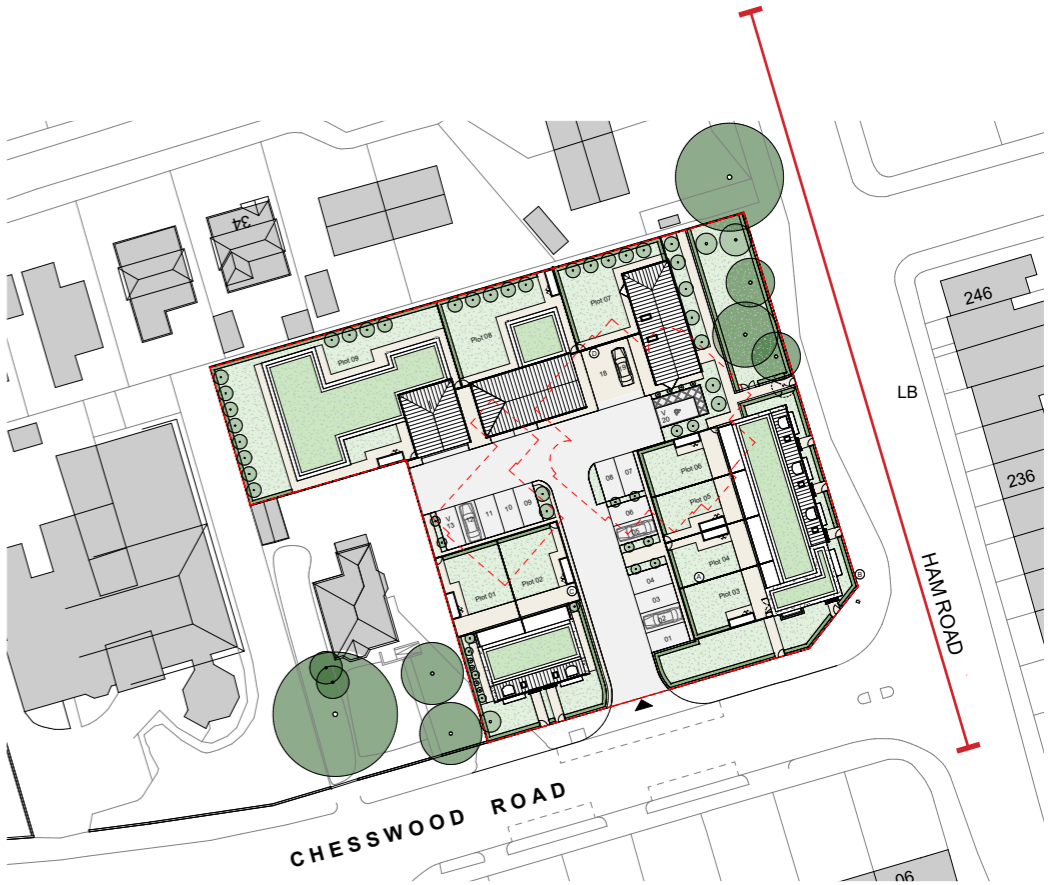
Section 2 through proposed House Type 5 and neighbouring building at Ashwood Close



Section 3 through proposed House Type 6 and neighbouring building at Ashwood Close

7.4. Scale and Design

The proposed material palette takes its cues from the 1960s church on site, drawing on its buff stonework and roof tones to create a respectful, contemporary response. The selected brickwork echoes the church's texture and warmth without direct imitation, fostering continuity between old and new. Similarly, the zinc cladding reflects the church's subdued roof tones, offering a modern yet sympathetic solution. Overall, the design acknowledges the site's ecclesiastical heritage while delivering a refined, contemporary residential character.



Following advice from planning officers, the scale and height of the end unit have been increased to create a corner-turning presence at the junction of Chesswood Road and Ham Road



Street Scene 1 - Ham Road

The new proposal follows the established typology of Ham Road, featuring a series of terraced houses with bay fronts and rooms within the roof space.

7.5. Scale and Design

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Street Scene 2 - Chesswood Road

The new proposal follows the established typology of Chesswood Road, featuring a series of semi-detached and detached houses with bay fronts and rooms within the roof space.

Following advice from planning officers, the scale and height of the end unit have been increased to create a corner-turning presence at the junction of Chesswood Road and Ham Road